

FERROUS MATERIALS REQUIRED

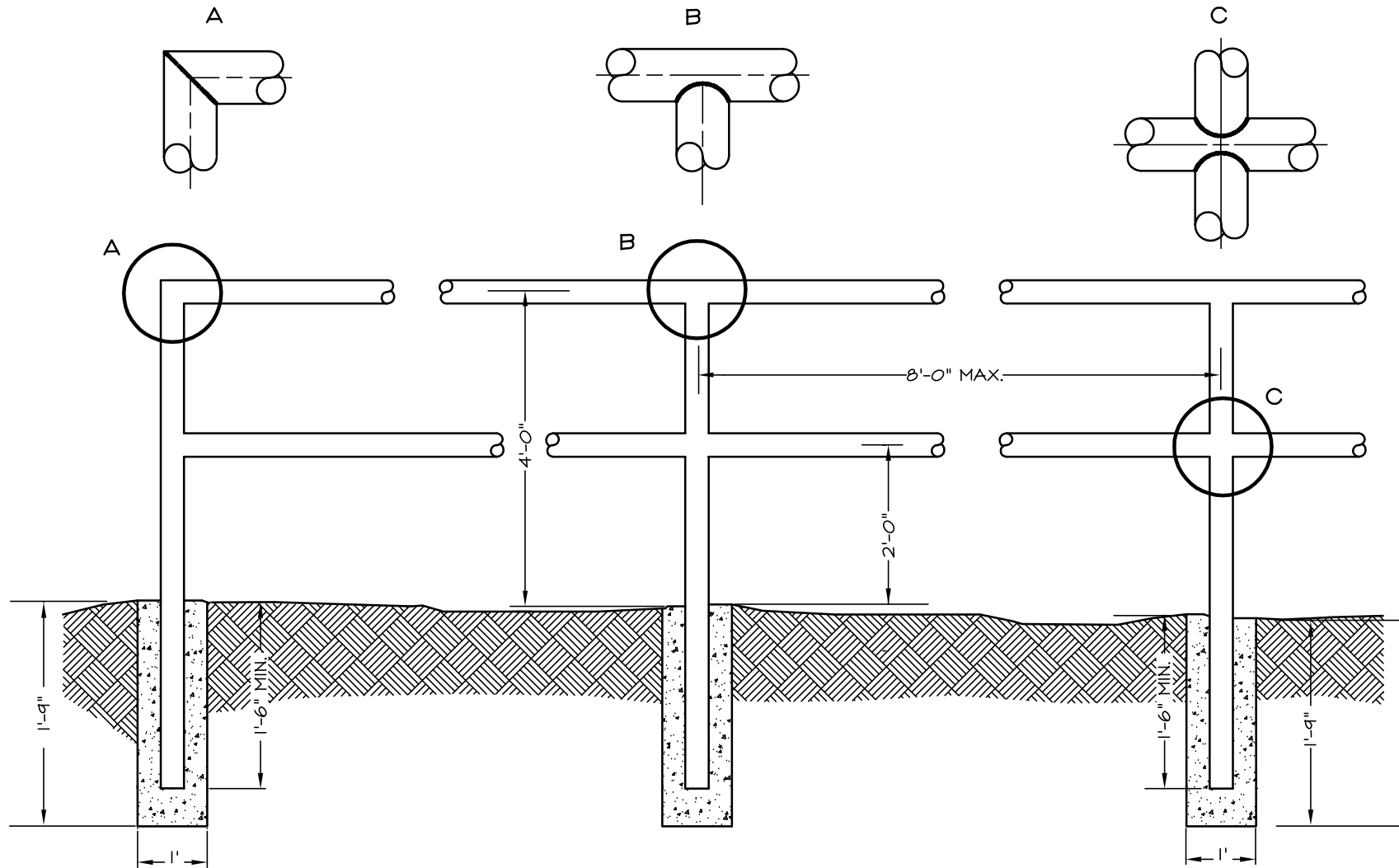
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**TOWN OF STALLINGS**  
**LAND DEVELOPMENT STANDARDS**

**TYPICAL CONCRETE CONTROL MONUMENT**

STD. NO.	REV.
50.03	



**GENERAL NOTES:**

1. ALL CONCRETE TO BE 3600 P.S.I. COMPRESSIVE STRENGTH.
2. TYPE OF PIPE TO BE USED IS 1-5/8" MAX. O.D. BLACK IRON, LOW CARBON PIPE OR GALVANIZED.
3. ALL JOINTS TO HAVE A 1/2" FILLET WELD AT ALL JOINTS.
4. AFTER INSTALLATION PAINT ASSEMBLY WITH BLACK ALL WEATHER ENAMEL.
5. SEE DETAIL 50.04-B FOR WARRANTS

NOT TO SCALE



**TOWN OF STALLINGS**  
**LAND DEVELOPMENT STANDARDS**

**SAFETY RAIL**

STD. NO.	REV.
50.04A	

## WARRANTS

STANDARD SAFETY RAIL (STD. #50.04A) SHALL BE INSTALLED UNDER ANY OF THE FOLLOWING CIRCUMSTANCES IN BOTH NEW CONSTRUCTION AND IN RETROFITTING OR RECONSTRUCTION OF EXISTING ROADWAYS OR SITES:

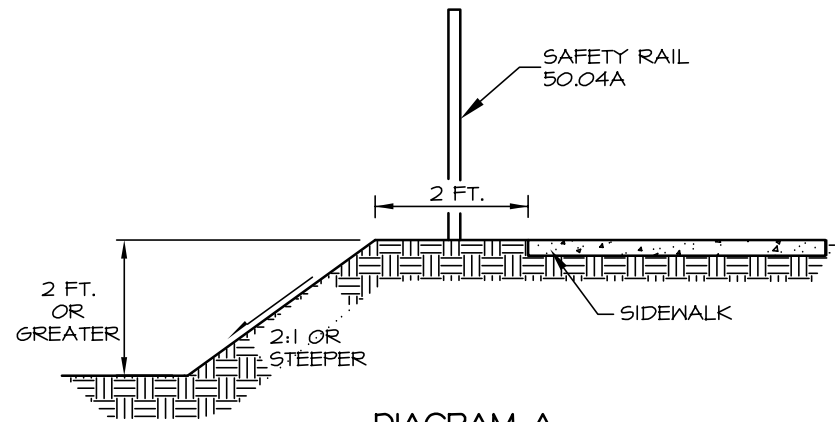
1. WHEN THE CULVERT CROSSING DETAIL (STD. #10.36A-B) APPLIES.
2. IF THERE IS A TWO FOOT OR GREATER DROPOFF WITHIN 2 FEET OF THE EDGE OF THE SIDEWALK (SEE DIAGRAM A).
3. IF THERE IS A 1-FOOT OR LARGER DROPOFF DIRECTLY ADJACENT TO THE SIDEWALK EDGE (SEE DIAGRAM B).
4. AT THE TOP OF ANY DROPOFF WITHIN THE PEDESTRIAN CLEAR ZONE OR WHERE PEDESTRIANS CAN REASONABLY BE EXPECTED IN THE VICINITY.
5. AT THE DIRECTION OF THE TOWN ENGINEER BASED ON FIELD CONDITIONS.

## DEFINITIONS

**DROPOFF** -- A SLOPE OF 2:1 OR STEEPER. EXAMPLES INCLUDE HEADWALLS, RETAINING WALLS, AND CULVERTS.

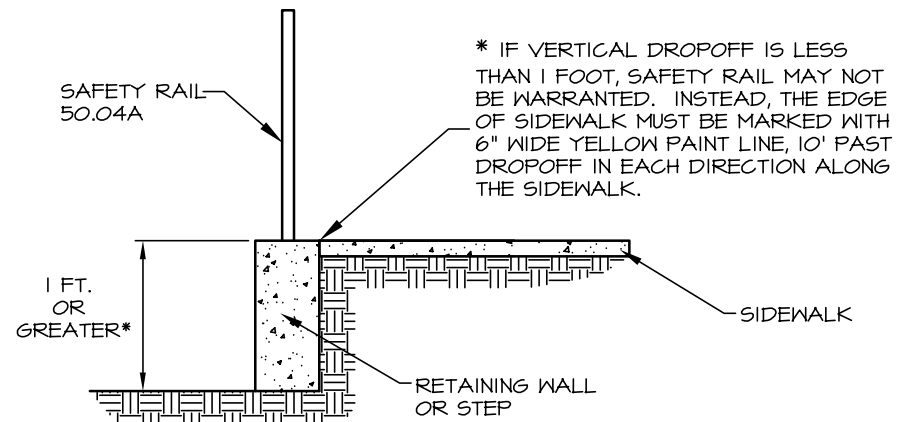
**PEDESTRIAN CLEAR ZONE** -- 10 FEET OF ANY COMBINATION OF SIDEWALK, SLOPE, AND SHOULDER SLOPED AT 6:1 OR FLATTER. SIDEWALK DOES NOT NEED TO BE PRESENT.

**SIDEWALK** -- FOR PURPOSES OF THIS STANDARD, THE TERM "SIDEWALK" IS USED GENERICALLY AND SHALL MEAN ANY PATH OR SURFACE TO BE USED FOR BICYCLE AND/OR PEDESTRIAN TRANSPORTATION. EXAMPLES INCLUDE, BUT ARE NOT LIMITED TO, SIDEWALKS, BIKE PATHS, SHARED-USE PATHS, PEDESTRIAN PATHS, AND GREENWAYS.



**DIAGRAM A**

SLOPED DROPOFF AT BACK OF SIDEWALK



**DIAGRAM B**

VERTICAL DROPOFF AT BACK OF SIDEWALK

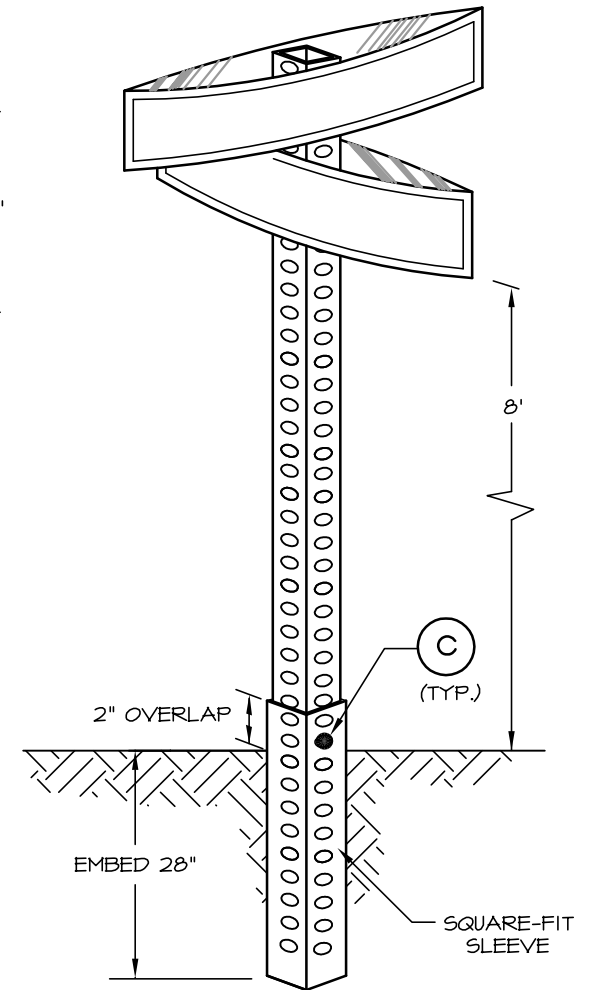
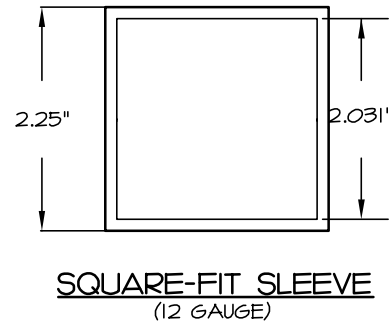
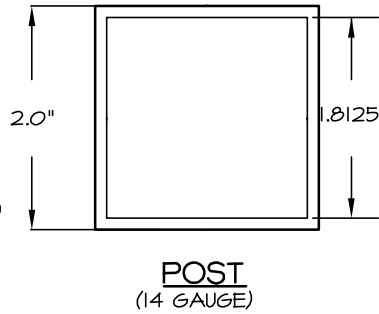
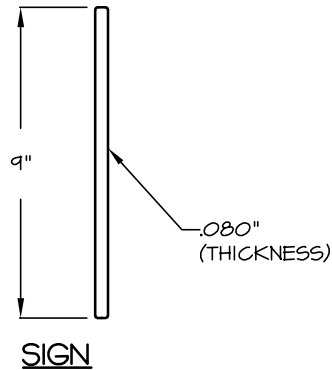
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**TOWN OF STALLINGS**  
**LAND DEVELOPMENT STANDARDS**

## SAFETY RAIL WARRANTS

STD. NO.	REV.
50.04B	

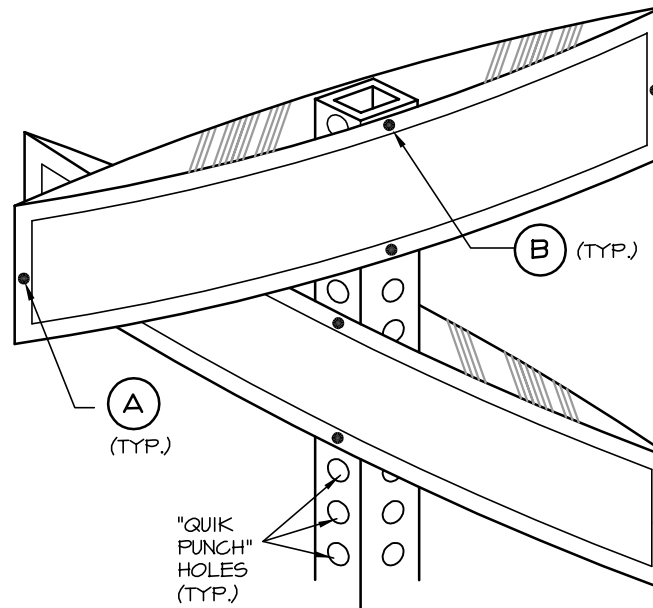


**KEY TO FASTENERS:**

- (A) #10-24 x 3/4" HEX HEAD MACHINE, ZINC- DEAD END  
 #10-24 FLANGE NUT, ZINC- DEAD END
- (B) 5/16" #16 X 3" CARRIAGE BOLT, ZINC  
 5/16" #16 HEX NUT, STEEL
- (C) 5/16" #16 X 2-3/4" CORNER BOLT (BREAKAWAY), ZINC  
 5/16" #16 HEX NUT, STEEL

**NOTES :**

1. POST SHALL BE 14-GAUGE GALVANIZED STEEL, QUIK-PUNCH, 1/16" HOLES, 1" ON CENTER, ALIGNED ON ALL SIDES, AND 2" SQUARE, 10 FEET IN LENGTH.
2. THE SLEEVE SHALL BE 12-GAUGE GALVANIZED STEEL, 1/16" HOLES, 1" ON CENTER, ALIGNED ON ALL SIDES, AND 2.25" SQUARE, 30" IN LENGTH.
3. ALL STREET NAME SIGNS ARE SUBJECT TO THE APPROVAL OF THE TOWN PLANNER OR HIS DESIGNEE.



NOT TO SCALE



**TOWN OF STALLINGS**  
**LAND DEVELOPMENT STANDARDS**

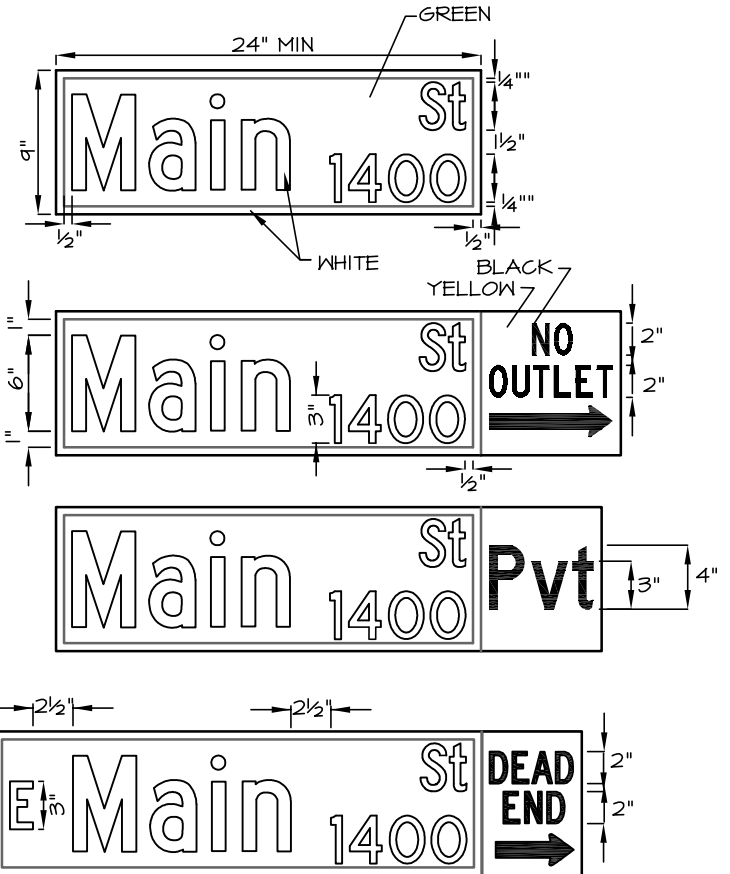
**STREET NAME SIGN**

STD. NO.	REV.
50.05A	

**NOTES**

- STREET NAME MARKERS (SNM) SHALL BE ALUMINUM, FLAT, AND HAVE DIMENSIONS AS SHOWN ON THIS DETAIL. MINIMUM LENGTH OF 24"; MAXIMUM LENGTH OF 60". THE SNM'S SHALL BE COVERED WITH WHITE HIGH INTENSITY PRISMATIC (HIP) RETRO-REFLECTIVE SHEETING (3M SERIES 3930 OR EQUIVALENT) WITH PRESSURE SENSITIVE ADHESIVE (OR EQUIVALENT TYPE IV OR HIGHER).
- THE LETTERS SHALL BE REVERSE CUT FROM TRANSPARENT GREEN OVERLAY FILM (3M #1177 EC FILM OR EQUIVALENT MEETING FEDERAL SPECIFICATION FP-96, SECTION 178.01(A) AND ASTM D4956). THE TRANSPARENT GREEN OVERLAY FILM MUST BE PLACED ON THE SNM TO PROVIDE AN EXPOSED 0.5" BORDER OF THE UNDERLAY WHITE HIP RETRO-REFLECTIVE SHEETING.
- THE STREET NAME SHALL BE COMPOSED OF INITIAL UPPER CASE LETTERS 6" IN HEIGHT AND CORRESPONDING LOWER CASE LETTERS 4.5" IN HEIGHT, IN FHWA "HIGHWAY B" FONT. THE STREET NAME SHALL BE LEFT-JUSTIFIED AND PLACED 0.5" FROM THE SIGN BORDER. ANY STREET NAME WITH 3 OR FEWER LETTERS SHALL BE CENTERED IN THE SIGN TEXT AREA.  

PREFIX/SUFFIX NAMES SHALL BE COMPOSED OF INITIAL UPPER CASE LETTERS 3" IN HEIGHT AND CORRESPONDING LOWER CASE LETTERS 2.25" IN HEIGHT, IN FHWA "HIGHWAY C" FONT.  
 BLOCK NUMBERS SHALL BE 3" IN HEIGHT, IN FHWA "HIGHWAY C" FONT.  
 SUFFIX NAMES AND BLOCK NUMBERS SHALL BE RIGHT-JUSTIFIED AND PLACED 0.5" FROM THE RIGHT-SIDE SIGN BORDER AND 0.25" FROM THE TOP AND BOTTOM SIGN BORDERS. PREFIX LETTERS (N, S, E, AND W) SHALL BE CENTERED AND PLACED 0.5" FROM THE LEFT-SIDE SIGN BORDER WITH 2.5" SPACING TO BEGINNING OF STREET NAME.
- SUPPLEMENTAL SNM WORDING ON YELLOW HIP RETRO-REFLECTIVE SHEETING WITH BLACK VINYL LETTERS SHALL BE PLACED ADJACENT TO THE GREEN OVERLAY FILM/BORDER TO INDICATE STREETS THAT DEAD END, HAVE NO OUTLET, ETC. OR ARE PRIVATE STREETS (PVT). THE YELLOW HIP RETRO-REFLECTIVE SHEETING MUST BE PLACED ON THE SNM TO MAINTAIN AN EXPOSED 0.5" BORDER OF THE UNDERLAY WHITE HIP RETRO-REFLECTIVE SHEETING.



NO OUTLET WITH ARROW (RIGHT OR LEFT) - PLACED ON SNM AT ENTRANCE TO A STREET OR STREET NETWORK FROM WHICH THERE IS NO OTHER EXIT. USE UPPER CASE LETTERS 2" IN HEIGHT, IN FHWA "HIGHWAY C" FONT.

PVT - PLACED ON SNM AT ENTRANCE TO PRIVATE STREET, USE UPPER CASE LETTER 4" IN HEIGHT AND CORRESPONDING LOWER CASE LETTERS 3" IN HEIGHT, IN FHWA "HIGHWAY C" FONT.

DEAD END WITH ARROW (RIGHT OR LEFT) - PLACED ON SNM AT ENTRANCE TO A SINGLE STREET THAT TERMINATES IN A DEAD END OR CUL-DE-SAC. USE UPPER CASE LETTERS 2" IN HEIGHT, IN FHWA "HIGHWAY C" FONT. IF STUB STREET IS LESS THAN OR EQUAL TO 200 FEET, THEN DEAD END IS NOT NECESSARY.

- ALL SNMs ARE SUBJECT TO THE APPROVAL OF THE TOWN PLANNER OR HIS DESIGNEE.

NOT TO SCALE

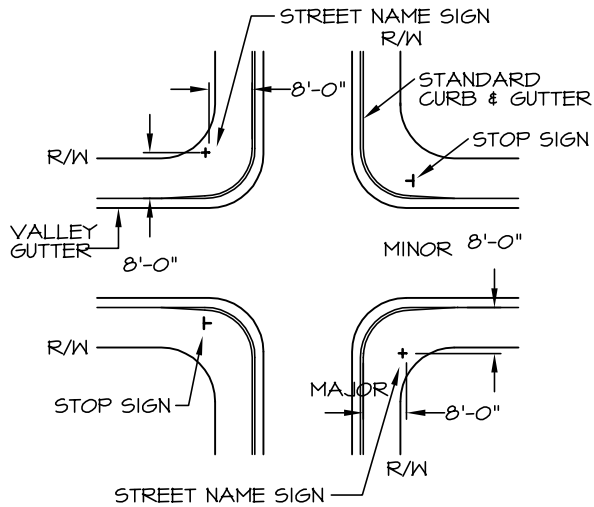


**TOWN OF STALLINGS**  
**LAND DEVELOPMENT STANDARDS**

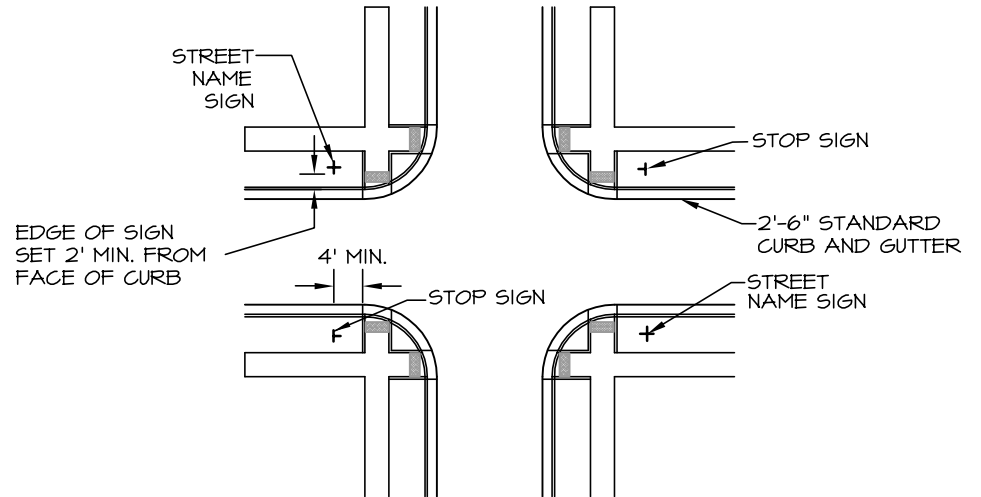
**STREET NAME SIGN**

STD. NO.	REV.
50.05B	

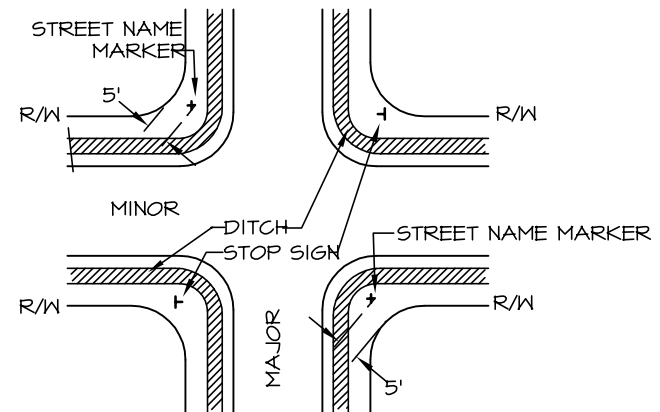
INTERSECTION with CURB and GUTTER



INTERSECTION with SIDEWALK, CURB, and GUTTER



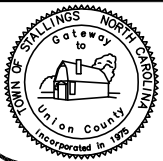
INTERSECTION with DITCHES, and NO CURB and GUTTER



NOTES

1. TWO STREET NAME MARKERS ARE REQUIRED IF THE MAJOR STREET HAS 3 OR MORE LANES.
2. ANY VARIANCE FROM THIS STANDARD MUST BE APPROVED BY THE TOWN PLANNER

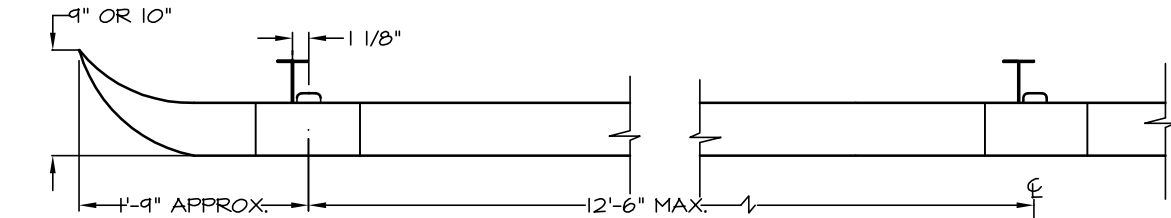
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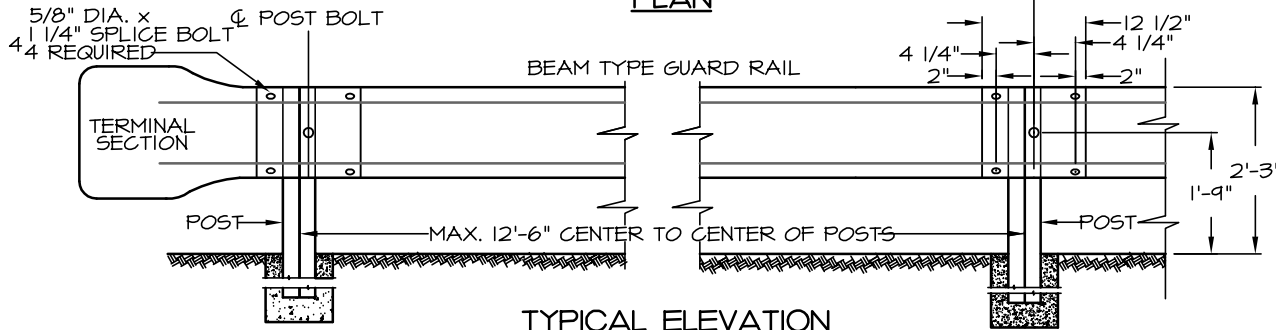
**TOWN OF STALLINGS**  
**LAND DEVELOPMENT STANDARDS**

**STREET NAME SIGN INSTALLATION LOCATIONS**

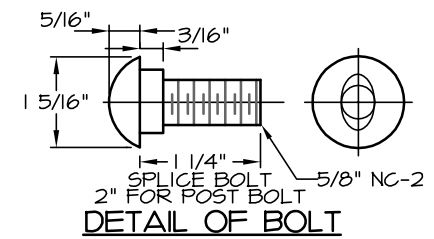
STD. NO.	REV.
50.06	



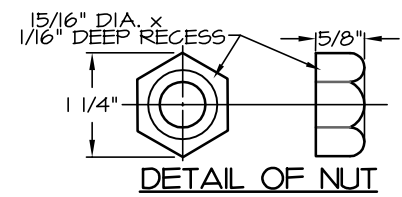
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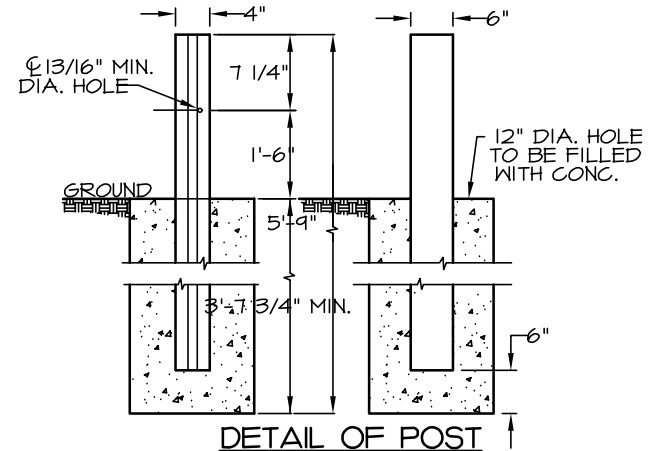
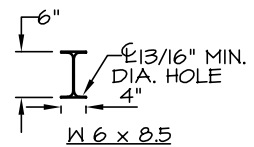
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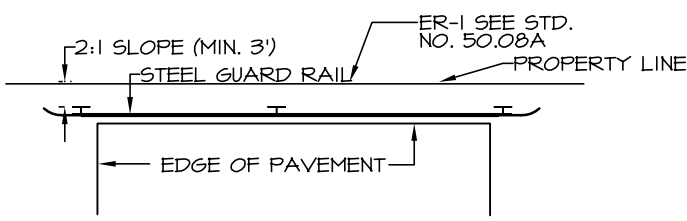
DETAIL OF BOLT



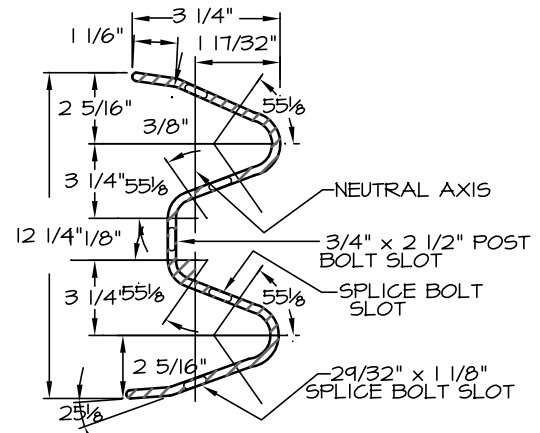
DETAIL OF NUT



DETAIL OF POST



DEAD-END STREET BARRICADE



SECTION THRU RAIL ELEMENT

**NOTE**  
THIS DETAIL IS NOT A GUARDRAIL DETAIL. FOR ROADSIDE GUARDRAIL, SEE NCDOT STANDARD DRAWINGS 862.01-862.03

NOT TO SCALE



**TOWN OF STALLINGS**  
**LAND DEVELOPMENT STANDARDS**

**DEAD END STREET BARRICADE**

STD. NO.	REV.
50.07A	

**GENERAL NOTES:**

1. STEEL BEAM TYPE GUARD RAILS SHALL BE INSTALLED AT THE END OF ALL DEAD-END STREETS, EXCEPT CUL-DE-SAC STREETS WHICH HAVE BEEN IMPROVED WITH A PERMANENT TURN-AROUND.
2. FOR STREETS 26' IN WIDTH THE GUARD RAIL SHALL CONSIST OF TWO(2) 12'-6" SECTIONS OR ONE(1) 25' SECTION, THREE (3) STEEL POSTS, AND TWO (2) TERMINAL SECTIONS. FOR STREETS GREATER THAN 25' IN WIDTH THE GUARD RAIL SHALL SPAN THE ENTIRE WIDTH OF THE STREET.
3. GUARD RAIL SHALL CONSIST OF RAIL ELEMENTS FABRICATED TO DEVELOP CONTINUOUS BEAM STRENGTH AND INSTALLED AS SHOWN.
4. MINIMUM THICKNESS OF GUARD RAIL SHALL BE 12 GAGE U.S. STANDARD.  
THE RAIL ELEMENT INCLUDING SPLICES, SHALL HAVE A MINIMUM ULTIMATE TENSILE STRENGTH OF 80,000 LBS.  
GUARD RAIL PARTS FURNISHED SHALL BE INTERCHANGEABLE WITH SIMILAR PARTS REGARDLESS OF THE SOURCE OF MANUFACTURER.  
THE HOLES FOR CONNECTING BOLTS SHALL BE PUNCHED OR DRILLED, BURNING WILL NOT BE PERMITTED.
5. THE GUARD, BOLTS, NUTS, STEEL POSTS, AND ALL OTHER METAL PARTS SHALL BE GALVANIZED TO CONFORM TO THE REQUIREMENTS FOR THE COATING CLASS, (2.50 OUNCES PER SQUARE FOOT) OF THE CURRENT SPECIFICATIONS FOR ZINC-COATED (GALVANIZED) IRON, AND STEEL SHEETS, COILS, AND CUT LENGTHS, IN ACCORDANCE WITH ASTM 123A.
6. IF THE AVERAGE SPELTER COATING AS DETERMINED FROM THE REQUIRED SAMPLES IS LESS THAN TWO (2) OUNCES OF SPELTER PER SQUARE FOOT, OR IF ANY ONE SPECIMEN HAS LESS THAN 1.8 ONCES OF SPELTER PER SQUARE FOOT OF DOUBLE EXPOSED SURFACE, THE LOT SAMPLED SHALL BE REJECTED, THE FINISHED SHEETS SHALL BE OF FIRST CLASS COMMERCIAL QUALITY, FREE FROM INJURIOUS DEFECTS, SUCH AS BLISTERS, FLUX, AND UNCOATED SPOTS.
7. THE GUARD RAIL SHALL BE INSPECTED TO DETERMINE THAT THE MATERIAL, DIMENSIONS, AND WORKMANSHIP ARE IN ACCORDANCE WITH THIS PLAN.
8. WHERE A DEAD-END STREET REQUIRES GUARD RAIL, END OF ROADWAY MARKER SIGNS SHALL ALSO BE REQUIRED.  
(SEE STD. 50.08A & 50.08B) (ER-1).

NOT TO SCALE

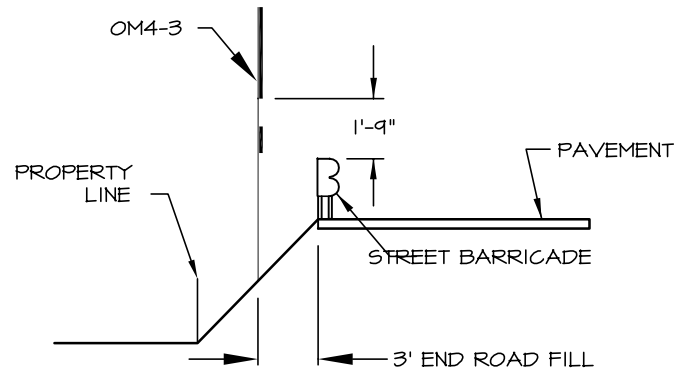


**TOWN OF STALLINGS**  
**LAND DEVELOPMENT STANDARDS**

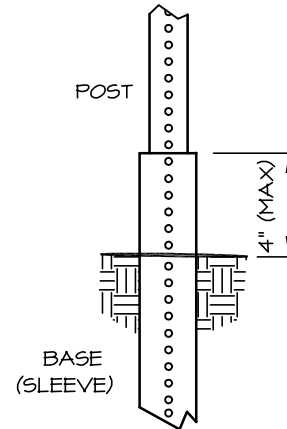
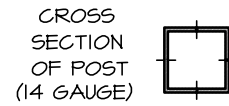
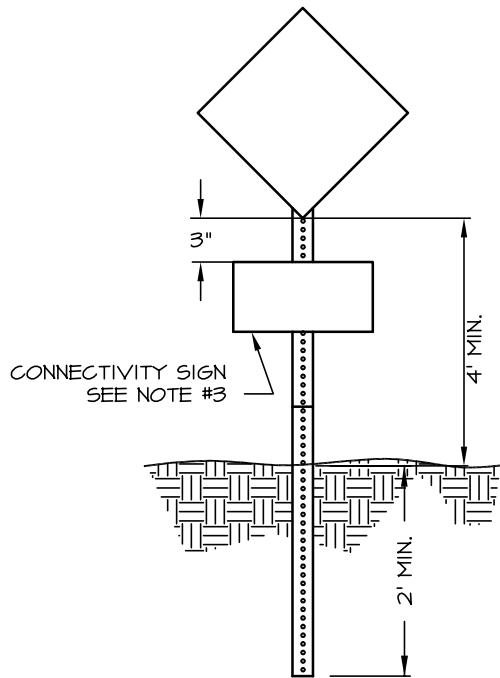
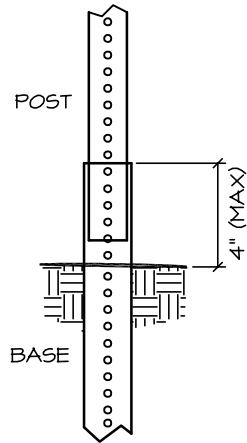
**DEAD END STREET BARRICADE**  
**GENERAL NOTES**

STD. NO.	REV.
50.07B	

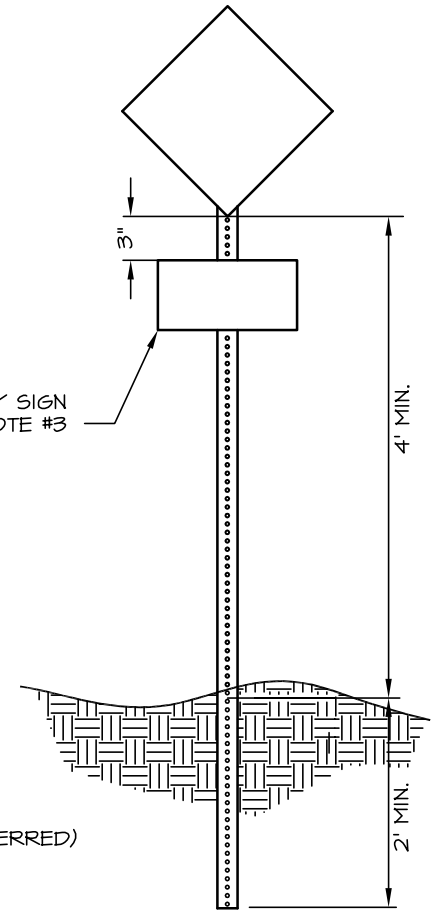




**SIGN LOCATION DETAIL**



CONNECTIVITY SIGN  
SEE NOTE #3



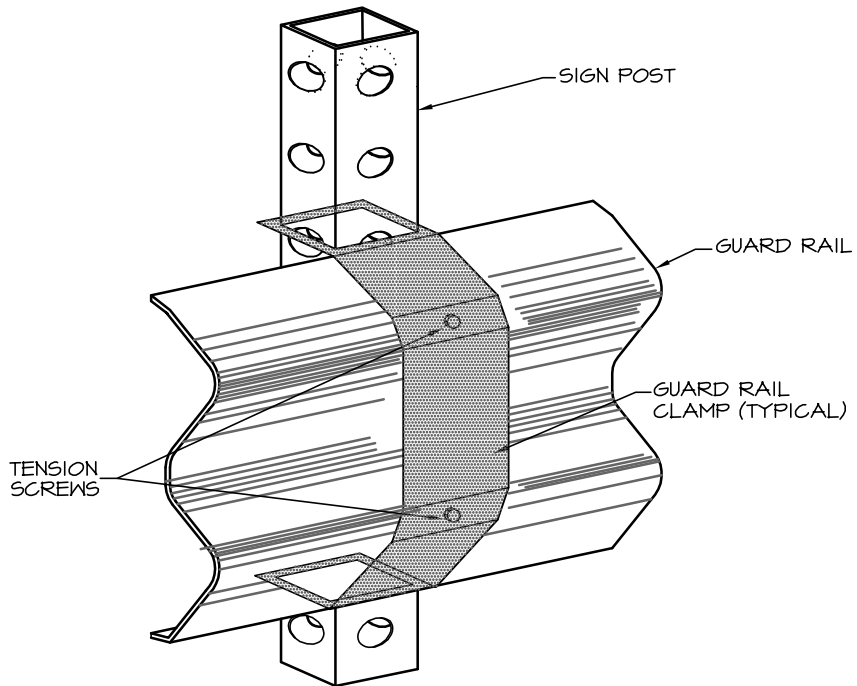
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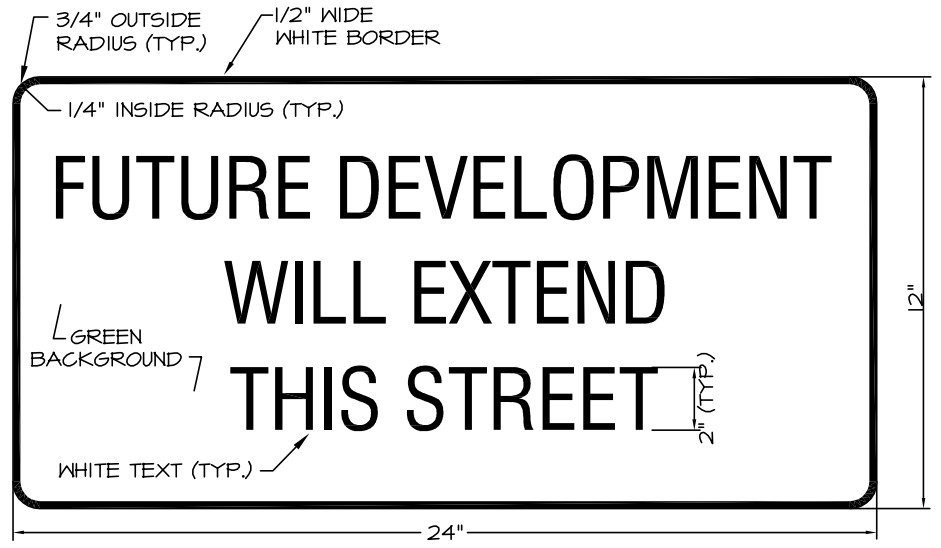
**TOWN OF STALLINGS  
LAND DEVELOPMENT STANDARDS**

**END OF ROADWAY MARKER**

STD. NO.	REV.
50.08A	



GUARD RAIL CLAMP INSTALLATION



STREET CONNECTIVITY SIGN

**NOTES:**

1. WHEN A DEAD-END OR STUBBED STREET REQUIRES A GUARDRAIL SECTION, END-OF-ROADWAY MARKER SIGNS (OM4-3, 24"x24", SOLID RED) SHALL BE PROVIDED.
2. SIGNS ARE TO BE PLACED BEHIND THE BARRICADE (SEE DETAILS 50.07A-B), EVENLY SPACED WITH ONE SIGN PLACED AT THE CENTERLINE LOCATION AND ADDITIONAL SIGNS AT 6' O.C. (MINIMUM OF 3 SIGNS, MAXIMUM OF 5 SIGNS).
3. WHEN BARRICADE IS USED ON A STREET STUB, THE SIGN AT THE CENTERLINE SHALL BE SUPPLEMENTED WITH A STREET CONNECTIVITY SIGN.
4. ALL SIGNS/MARKERS SHALL MEET OR EXCEED MUTCD STANDARDS FOR RETROREFLECTIVITY.

1. SIGN SHALL MEET OR EXCEED MUTCD STANDARDS FOR RETROREFLECTIVITY
2. SIGN MATERIAL SHALL BE 0.080" THICK ALUMINUM
3. ALL LETTERS SHALL BE SERIES B-2000 FROM THE 2004 STANDARD HIGHWAY SIGNS MANUAL (AND ANY REVISION THERETO) PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION.

NOT TO SCALE

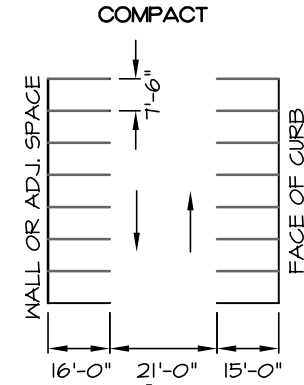
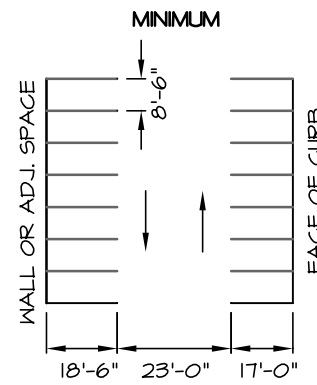
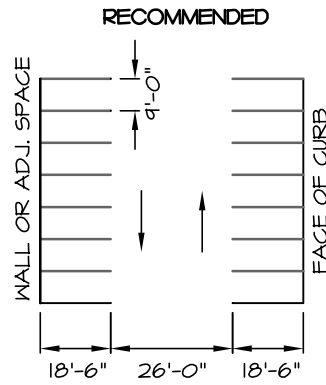


**TOWN OF STALLINGS**  
**LAND DEVELOPMENT STANDARDS**

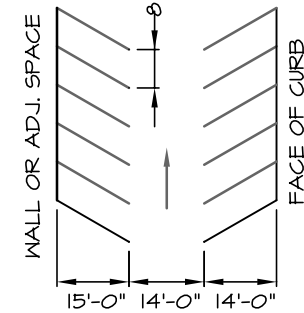
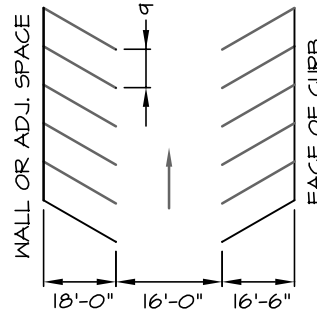
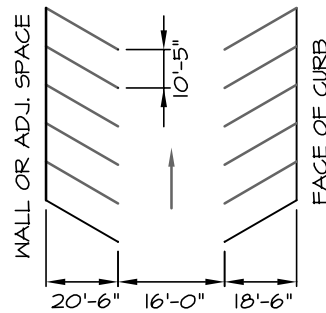
**END OF ROADWAY MARKER**

STD. NO.	REV.
50.08B	

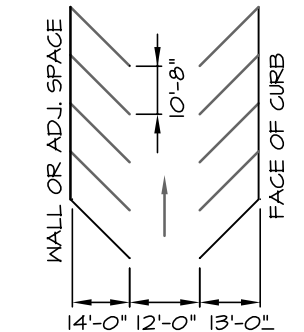
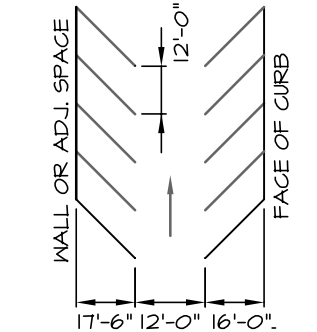
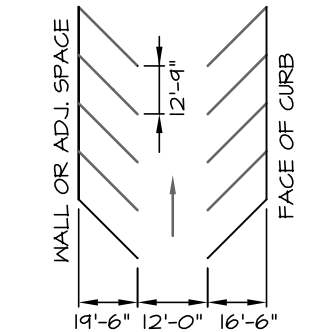
PARKING ANGLE 90°  
(TWO WAY OPERATION ONLY)



PARKING ANGLE 60°  
(ONE WAY OPERATION ONLY)



PARKING ANGLE 45°  
(ONE WAY OPERATION ONLY)



**NOTES:**

1. FOR ACCESSIBLE PARKING STANDARDS/SIGNAGE SEE STDS. 50.10A, B, AND C.
2. PAVEMENT MARKINGS SHALL BE 4" WHITE PAINT.
3. ALTERNATIVE PARKING ANGLES, AISLE WIDTHS, AND OPERATION (TWO-WAY ANGLED PARKING OR REVERSE-ANGLE PARKING) WILL BE CONSIDERED BY TOWN ENGINEER ON A CASE-BY-CASE BASIS.

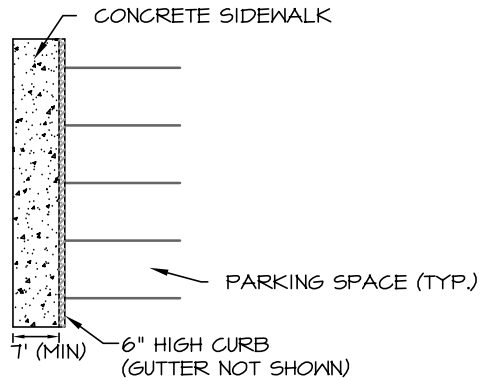
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**TOWN OF STALLINGS**  
**LAND DEVELOPMENT STANDARDS**

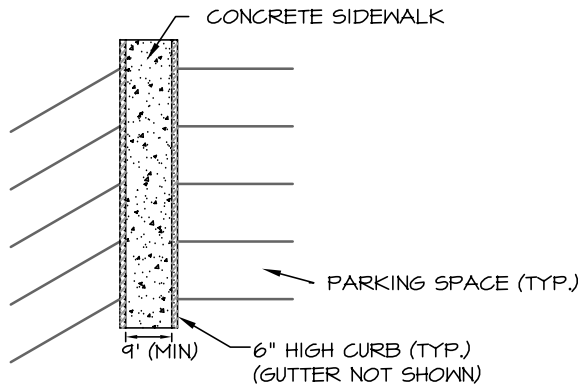
**PARKING STANDARDS**

STD. NO.	REV.
50.09A	



**PARKING ON ONE SIDE OF A SIDEWALK**

SIDEWALK ADJACENT TO HEAD-IN OR BACK-IN PARKING SHALL BE AT LEAST 7 FEET WIDE.

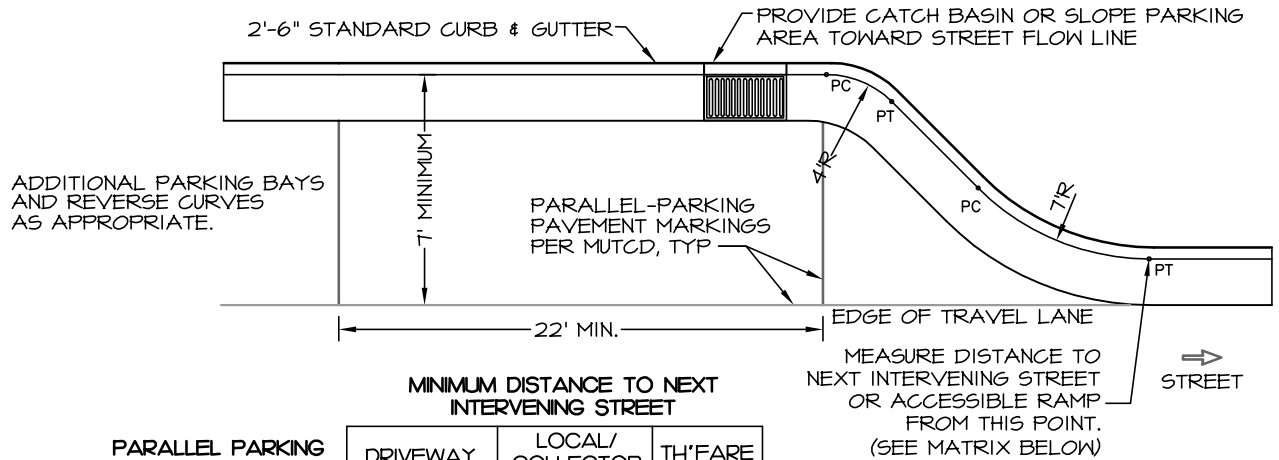


**PARKING ON BOTH SIDES OF A SIDEWALK**

SIDEWALK BETWEEN TWO ROWS OF HEAD-IN OR BACK-IN PARKING SHALL BE AT LEAST 9 FEET WIDE.

**NOTES:**

1. REVERSE CURVES NOT NECESSARY IF ADEQUATE DRAINAGE CAN BE PROVIDED THAT WILL ENSURE THAT SEDIMENT, WATER, DEBRIS, ETC., DOES NOT COLLECT IN 90-DEGREE CORNERS.
2. PARALLEL ACCESSIBLE SPACES AND LOADING ZONES TO BE REVIEWED ON A CASE-BY-CASE BASIS.
3. FOR PARKING BAYS THAT ARE 8 FEET IN WIDTH OR GREATER, THE PAVEMENT MARKINGS SHALL BE SET AT ONE (1) FOOT LESS THAN THE STALL WIDTH.
4. GREATER SEPARATION FROM INTERVENING STREETS THAN THE DISTANCES PROVIDED BELOW MAY BE REQUIRED AT THE TOWN ENGINEER'S DISCRETION.
5. POSITIVE DRAINAGE SHALL BE PROVIDED EITHER BY INSTALLATION OF APPROPRIATE DRAINAGE STRUCTURES OR SLOPE PARKING AREA TO STREET FLOW LINE. SLOPING PARKING AREA TO STREET FLOW LINE ONLY PERMITTED IF ROAD GRADE IS GREATER THAN 2%.
6. IF A BIKE LANE IS REQUIRED ADJACENT TO PARALLEL PARKING, THE MINIMUM WIDTH OF BIKE LANE IS 6'.



MINIMUM DISTANCE TO NEXT INTERVENING STREET

PARALLEL PARKING BAY LOCATED ON	DRIVEWAY	LOCAL/ COLLECTOR	TH'FARE
LOCAL/COLLECTOR	20'	20'	20'
THOROUGHFARE	20'	20'	50'

**PARALLEL PARKING STANDARDS**

NOT TO SCALE

**NOTES:**

1. A 2-FOOT-WIDE PLANTING STRIP LOCATED AT THE BACK OF CURB CAN BE USED IN LIEU OF 2 FEET OF SIDEWALK WIDTH.
2. PARKING AT ANY ANGLE OTHER THAN PARALLEL SHALL BE SUBJECT TO THIS STANDARD.
3. IF MONOLITHIC CURB & SIDEWALK IS USED, ADD 6" TO ALL DIMENSIONS (1' IF PARKING ON BOTH SIDES).
4. WHEELSTOPS SHALL ONLY BE USED IN LIEU OF 2 FEET OF SIDEWALK WITH THE APPROVAL OF THE TOWN AND WHEN EXISTING CONDITIONS PREVENT CONSTRUCTION OF A 7-FOOT/9-FOOT SIDEWALK. WHEELSTOPS SHALL BE 6" HIGH, MADE OUT OF 3600-PSI REINFORCED CONCRETE, AND ANCHORED WITH #5 OR GREATER REBAR (2' MINIMUM LENGTH). REBAR HOLES SHALL BE GROUTED UPON INSTALLATION. WHEELSTOPS SHALL BE PLACED AT 2 FEET FROM THE EDGE OF SIDEWALK OR OBSTRUCTION.



**TOWN OF STALLINGS**  
**LAND DEVELOPMENT STANDARDS**

**PARKING STANDARDS, CONT.**

STD. NO.	REV.
50.09B	

### ACCESSIBLE PARKING REQUIREMENTS

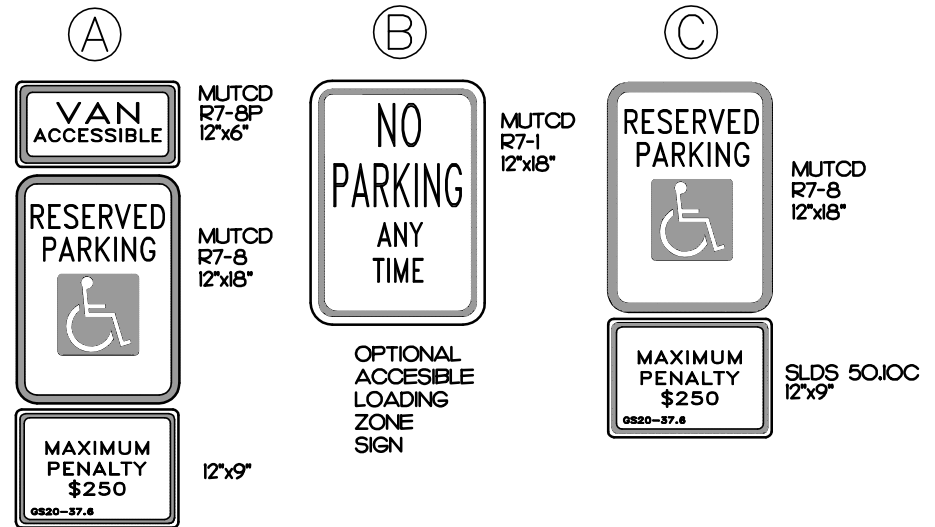
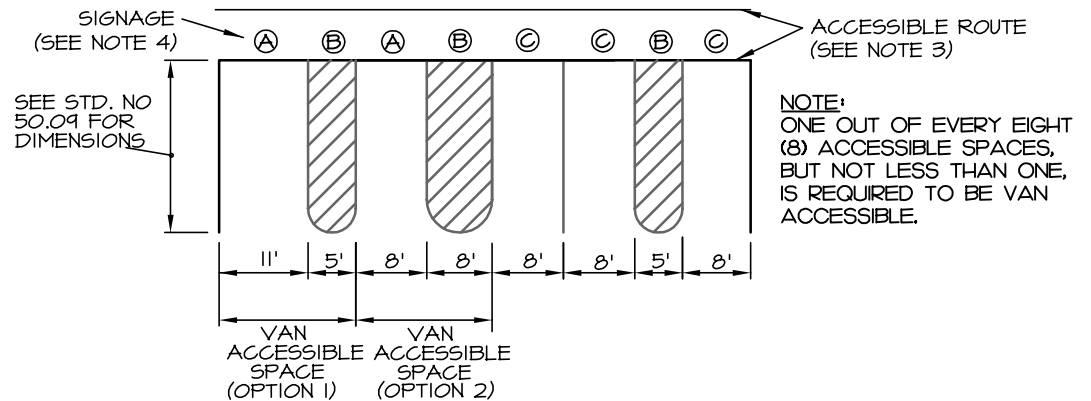
TOTAL PARKING SPACES PROVIDED	MINIMUM NUMBER OF ACCESSIBLE SPACES REQUIRED	MINIMUM NUMBER OF ACCESSIBLE SPACES REQUIRED TO BE VAN ACCESSIBLE
1 TO 25	1	1
26 TO 50	2	1
51 TO 75	3	1
76 TO 100	4	1
101 TO 150	5	1
151 TO 200	6	1
201 TO 300	7	1
301 TO 400	8	1
401 TO 500	9	2
501 TO 1000	2% OF TOTAL	1 IN EVERY 8 ACCESSIBLE SPACES
1001 AND OVER	20 PLUS 1 FOR EACH 100 OVER 1000	1 IN EVERY 8 ACCESSIBLE SPACES

REF.: SECTION 208 OF THE ADA STANDARDS FOR ACCESSIBLE DESIGN

### NOTES:

- ALL 12"x18" ACCESSIBLE SIGNS (R7-8 & R7-1) SHALL BE MOUNTED AT 7 FEET FROM GRADE TO BOTTOM EDGE OF SIGN FACE (MUTCD). MOUNTING HEIGHT CAN BE REDUCED TO 5 FEET IF PLACED IN AN AREA BETWEEN SIDEWALK AND BUILDING FACE IN WHICH PEDESTRIANS ARE NOT EXPECTED TO USE.
- REFER TO MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, (MUTCD) U.S. DEPARTMENT OF TRANSPORTATION AND NORTH CAROLINA DEPARTMENT OF TRANSPORTATION SUPPLEMENT.
- IF ACCESSIBLE ROUTE IS A RAISED SIDEWALK AREA, THEN RAMPS ARE REQUIRED AT LOADING ZONE AREA.
- SIGNAGE MUST NOT OBSTRUCT ACCESSIBLE ROUTE OR RAMPS.

### PARKING SPACE PAVEMENT MARKINGS



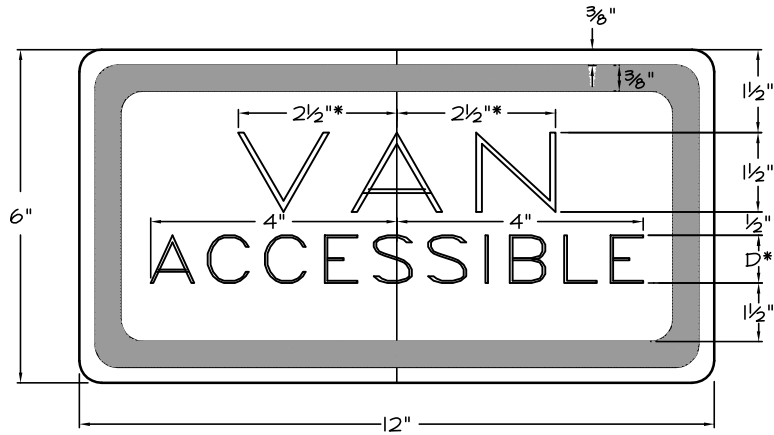
NOT TO SCALE



**TOWN OF STALLINGS**  
**LAND DEVELOPMENT STANDARDS**

## ACCESSIBLE PARKING AND SIGNAGE STANDARDS

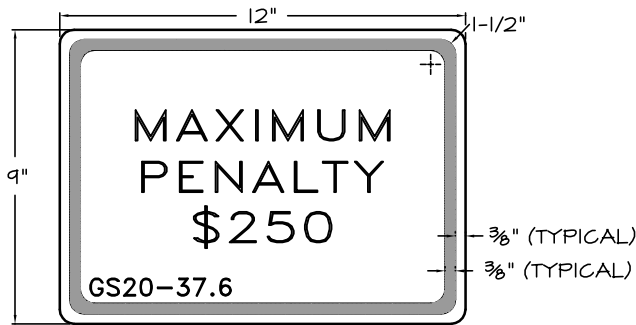
STD. NO.	REV.
50.10A	



LEGEND AND BORDER - GREEN  
BACKGROUND - WHITE

INCREASE SPACING 50% D-FHWA (FEDERAL HIGHWAY  
ADMINISTRATION/USDOT) SERIES D LETTERS

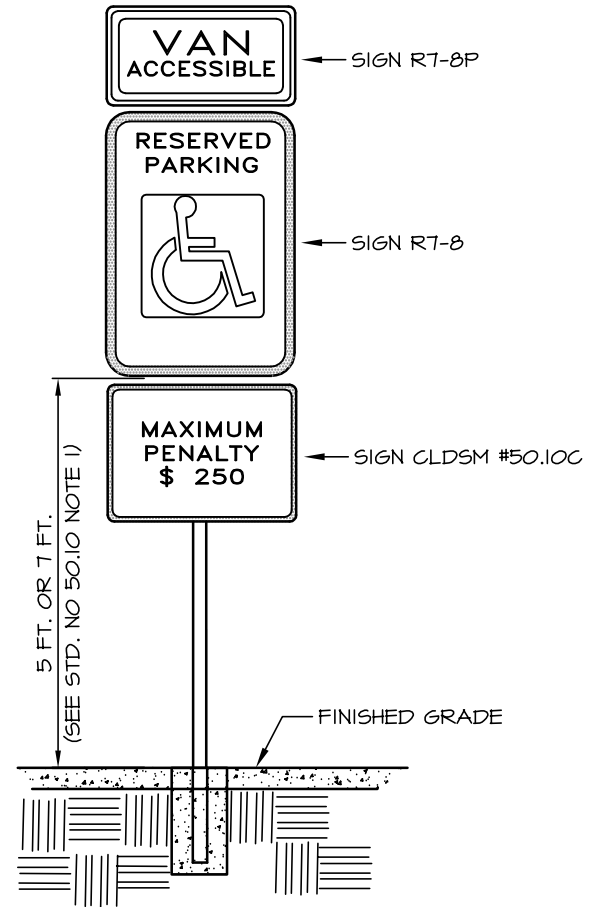
SUPPLEMENTAL VAN ACCESSIBLE  
SIGN (R7-8P)



LEGEND AND BORDER - GREEN  
BACKGROUND - WHITE

SIGN APPROVED FOR USE UNDER GENERAL STATUTE 20-37.6

THIS PENALTY SIGN IS REQUIRED TO ACCOMPANY ALL  
R7-8 PARKING SIGNS ERECTED AFTER DECEMBER  
31,1990



**NOTE:**

SUPPLEMENTAL VAN ACCESSIBLE SIGN (R7-8P) USED IF  
THERE IS ONLY ONE REQUIRED ACCESSIBLE PARKING  
SPACE (MUST BE VAN ACCESSIBLE) AND AT EACH  
ADDITIONAL REQUIRED VAN ACCESSIBLE SPACE. (SEE  
STD. NO. 50.10)

NOT TO SCALE



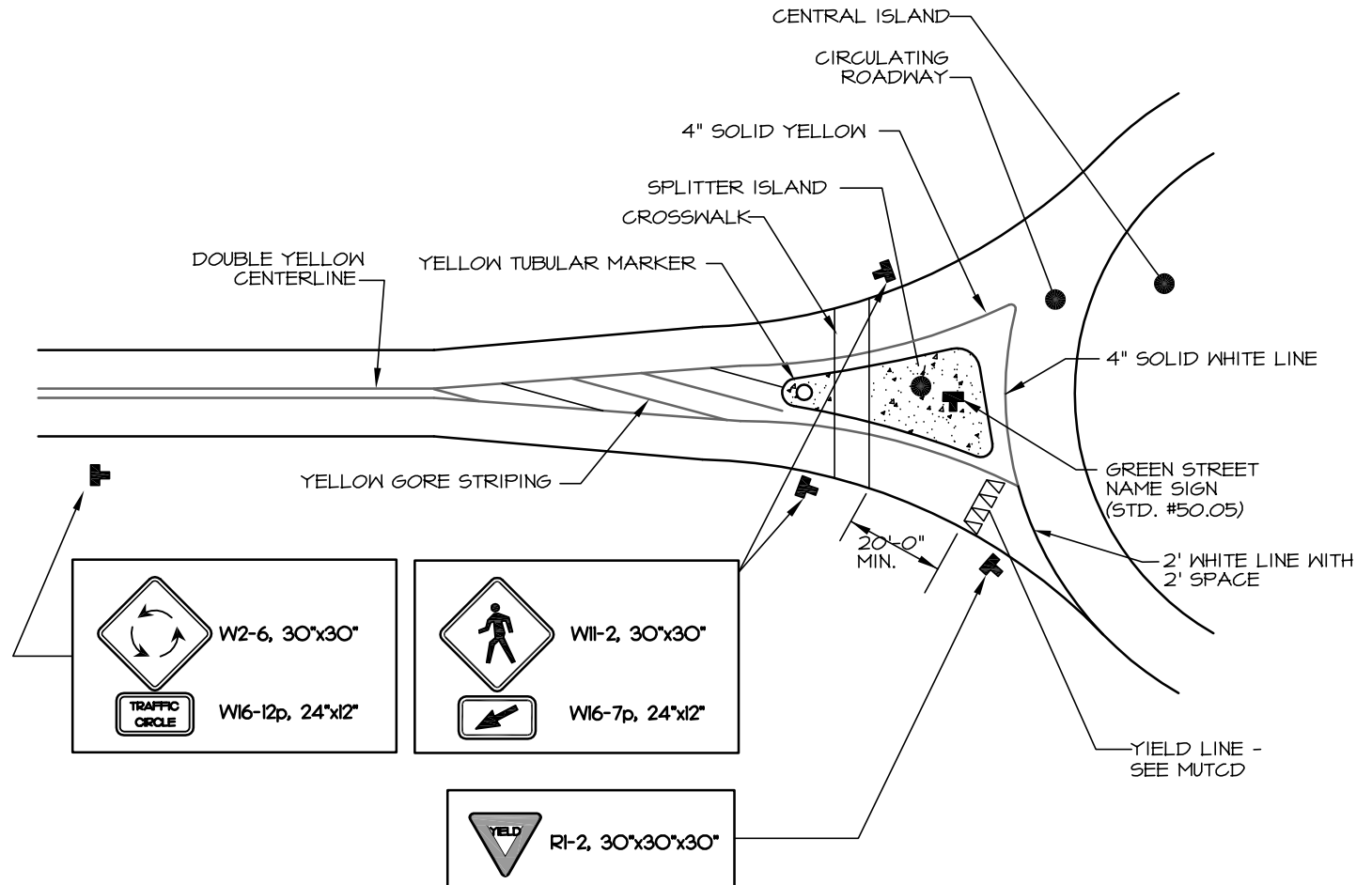
**TOWN OF STALLINGS**  
**LAND DEVELOPMENT STANDARDS**

**ACCESSIBLE PARKING**  
**SUPPLEMENTAL SIGNAGE**

STD. NO.	REV.
50.10B	

**NOTES:**

- PAVEMENT MARKINGS TO BE PER LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- SIGNS TO BE LOCATED/SPACED PER MUTCD REQUIREMENTS.
- "CIRCULAR INTERSECTION" AND "TRAFFIC CIRCLE" SUBPLATE SIGNS, AND YELLOW TUBULAR MARKERS, ARE REQUIRED ON THOROUGHFARES. CDOT WILL DETERMINE IF ONE OR MORE OF THESE ARE NECESSARY ON LOCAL OR COLLECTOR STREETS.
- "PEDESTRIAN CROSSING" AND ARROW SUBPLATE SIGNS ARE REQUIRED WHEREVER THERE IS A MARKED CROSSWALK OR ON A THOROUGHFARE.
- "YIELD" SIGNS ARE ALWAYS REQUIRED.
- PAVEMENT MARKINGS, SPLITTER ISLAND DESIGNS, CROSSWALK, ETC., ARE SHOWN FOR CONTEXT ONLY. REFER TO THE MUTCD AND/OR THE FEDERAL HIGHWAY ADMINISTRATION'S MANUAL ROUNDABOUTS: AN INFORMATIONAL GUIDE FOR MORE DETAIL OR DESIGN INFORMATION.
- ADDITIONAL SIGNS MAY BE NEEDED ON A CASE-BY-CASE BASIS, TO BE EVALUATED BY TOWN ENGINEER.
- ALL PAVEMENT MARKING SHALL BE THERMOPLASTIC.



NOT TO SCALE

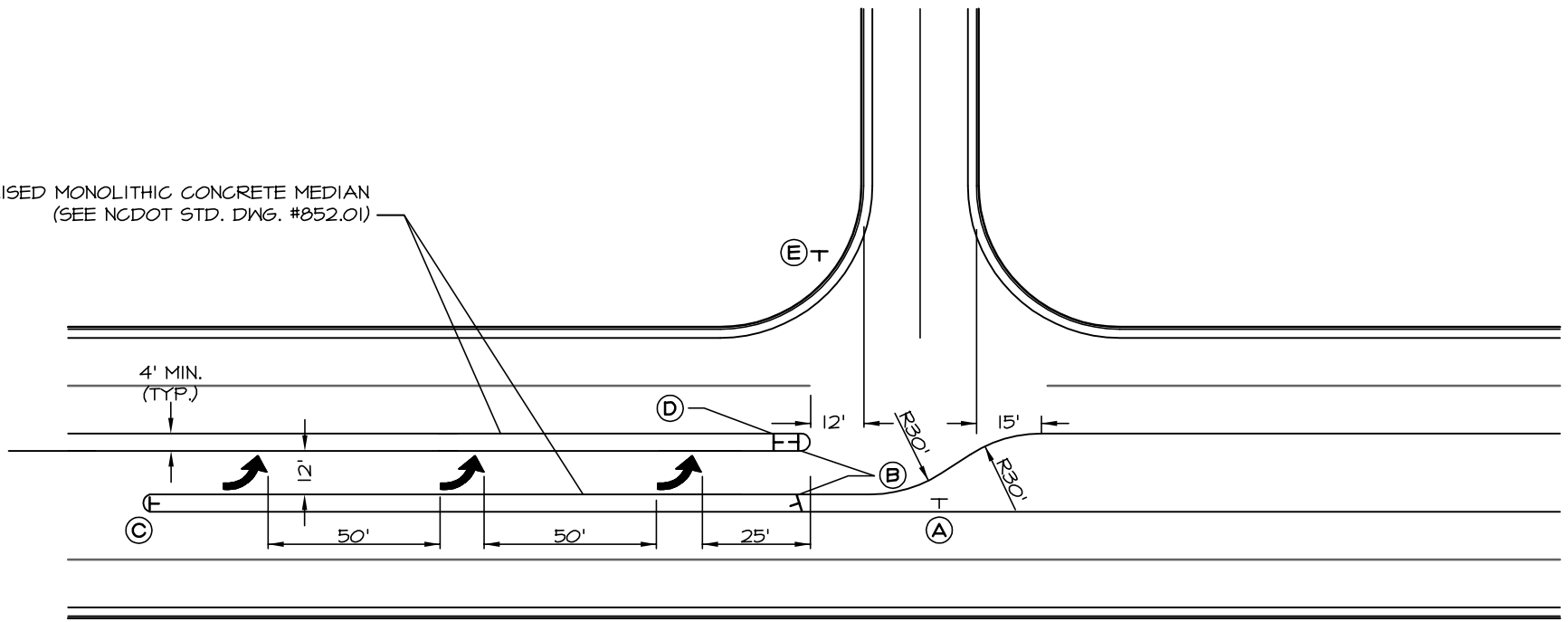


**TOWN OF STALLINGS**  
**LAND DEVELOPMENT STANDARDS**

**SIGNAGE AND PAVEMENT MARKINGS**  
**AT ROUNDABOUTS**

STD. NO.	REV.
50.11	

RAISED MONOLITHIC CONCRETE MEDIAN  
(SEE NCDOT STD. DWG. #852.01)



**NOTES:**

1. ADDITIONAL PAVEMENT MARKINGS (EDGE LINES, GORES, ETC.) ARE NOT SHOWN BUT ARE REQUIRED BY THE TOWN ENGINEER.
2. FOR DIVIDED SIDE STREETS, MEASURE THE 12 FOOT DIMENSION FROM THE FACE OF MEDIAN INSTEAD OF FACE OF CURB ON APPROACHING LANE.
3. ALL SIGNS SHALL BE MUTCD STANDARD SIGNS.

**SIGN LEGEND**

- (A) ONE WAY (R6-2R, 18"x24")
  - (B) DO NOT ENTER (R5-1, 30"x30")
  - (C) DOUBLE-DOWN ARROW (W12-1, 30"x30")
  - (D) NO U-TURN (R3-4, 24"x24")\*
  - (E) STOP (R1-1, 30"x30")
- \* IF NECESSARY

NOT TO SCALE

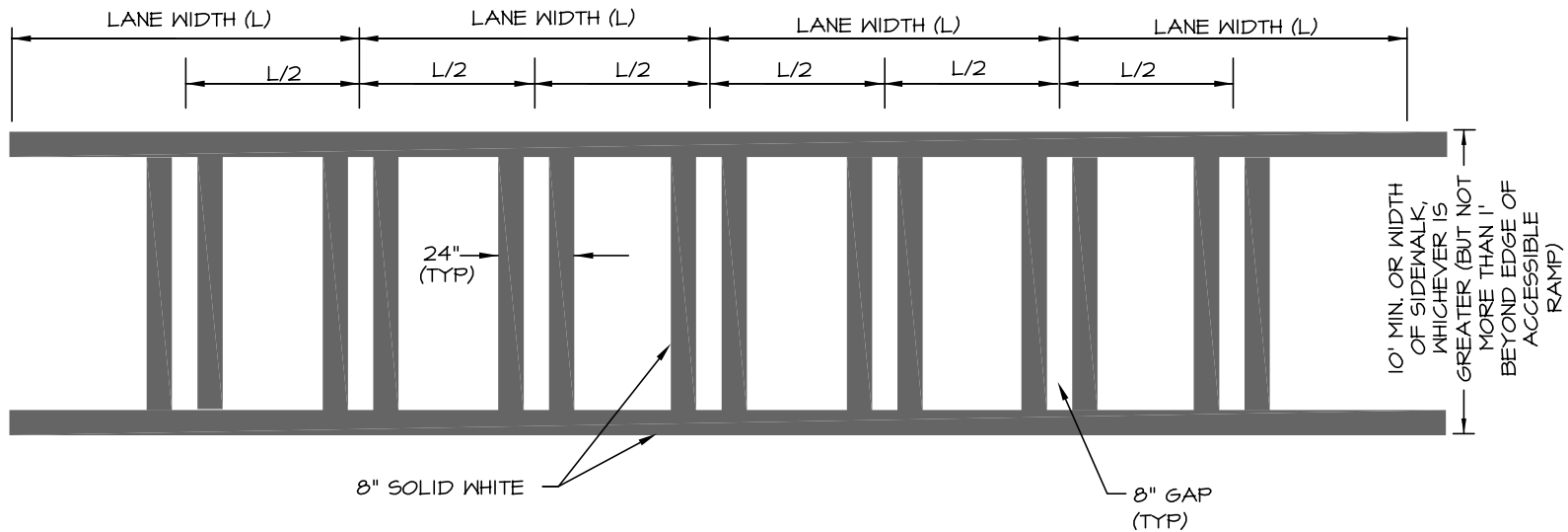


**TOWN OF STALLINGS  
LAND DEVELOPMENT STANDARDS**

**DIRECTIONAL CROSSOVER  
WITH RAISED MEDIANS**

STD. NO.	REV.
50.13	





**NOTES**

1. PER MUTCD STANDARDS, WHEN CROSSWALK LINES ARE USED THEY SHALL CONSIST OF SOLID WHITE LINES THAT MARK THE CROSSWALK. THEY SHALL BE NOT LESS THAN 150 MM (6 IN) NOR GREATER THAN 600 MM (24 IN) IN WIDTH.
2. IF TRANSVERSE LINES ARE USED TO MARK A CROSSWALK, THE GAP BETWEEN THE LINES SHOULD NOT BE LESS THAN 1.8 M (6 FT). IF DIAGONAL OR LONGITUDINAL LINES ARE USED WITHOUT TRANSVERSE LINES TO MARK A CROSSWALK, THE CROSSWALK SHOULD NOT BE LESS THAN 1.8 M (6 FT) WIDE.
3. IF USED, THE DIAGONAL OR LONGITUDINAL LINES SHOULD BE 300 TO 600 MM (12 TO 24 IN) WIDE AND SPACED 300 TO 1500 MM (12 TO 60 IN) APART. THE MARKING DESIGN SHOULD AVOID THE WHEEL PATHS, AND THE SPACING SHOULD NOT EXCEED 25 TIMES THE LINE WIDTH.

NOT TO SCALE



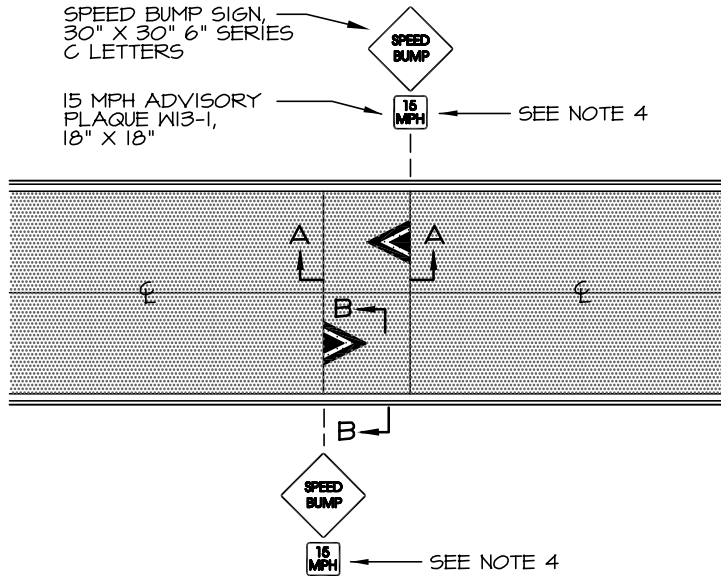
**TOWN OF STALLINGS**  
**LAND DEVELOPMENT STANDARDS**

**PIANO-STYLE CROSSWALK**

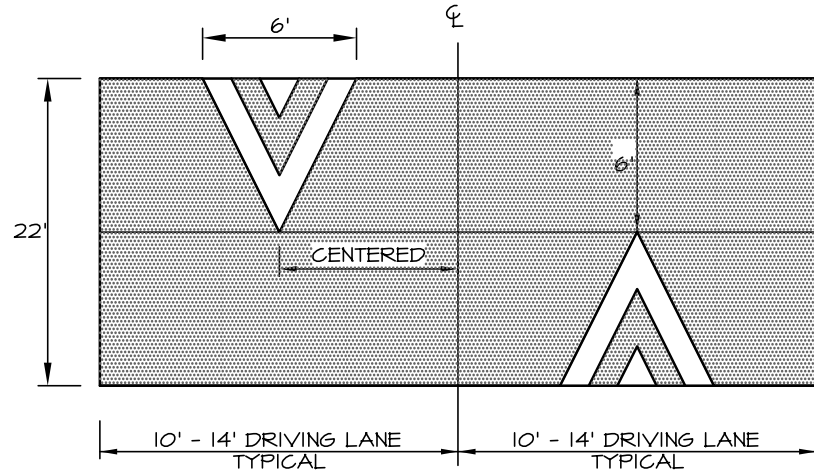
STD. NO.	REV.
50.14	

SPEED BUMP SIGN,  
30" X 30" 6" SERIES  
C LETTERS

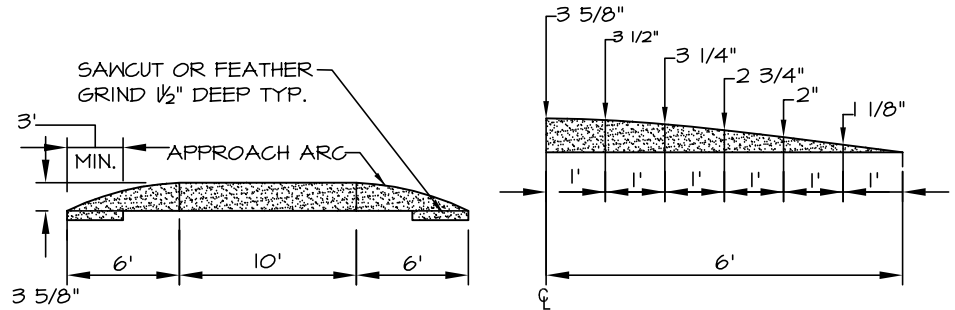
15 MPH ADVISORY  
PLAQUE W13-1,  
18" X 18"



**SPEED HUMP MARKING AND SIGNING**

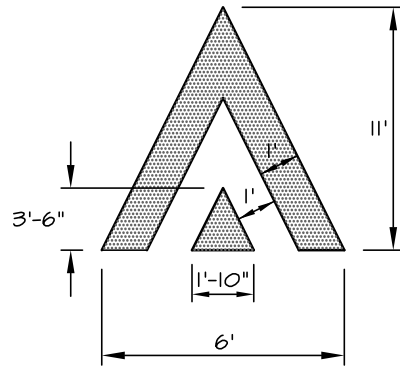


**MARKING DETAIL**

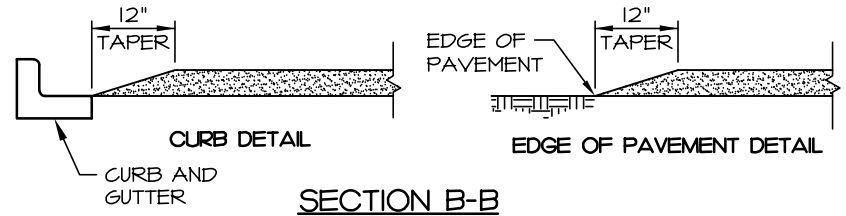


**SECTION A-A**

**APPROACH ARC DETAIL**



**CHEVRON DETAIL**



**CURB DETAIL**

**EDGE OF PAVEMENT DETAIL**

**SECTION B-B**

**NOTES:**

1. SAWCUT OR FEATHER GRIND TO KEY IN SPEED HUMP. SEE SECTION A-A.
2. SIGN LOCATIONS SHALL BE VERIFIED BY THE ENGINEER PRIOR TO INSTALLATION.
3. SPEED HUMP CHEVRON MARKING SHALL BE THERMOPLASTIC, HEAT FUSED PREFORMED, 90 MIL., OR EQUAL APPROVED BY THE ENGINEER.
4. FOR A SERIES OF SPEED HUMPS, USE THE ADVISORY SPEED PLAQUE AT ONLY THE FIRST SPEED HUMP IN EACH DIRECTION OF TRAVEL.
5. FOR A SERIES OF SPEED HUMPS, INSTALL W17-1 SIGN ONLY AT THE ENTRANCES OF A SUBDIVISION.
6. USE PAVEMENT TYPE S9.5B

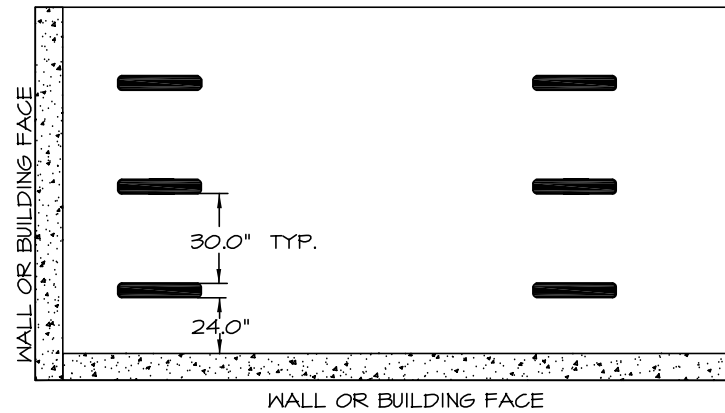
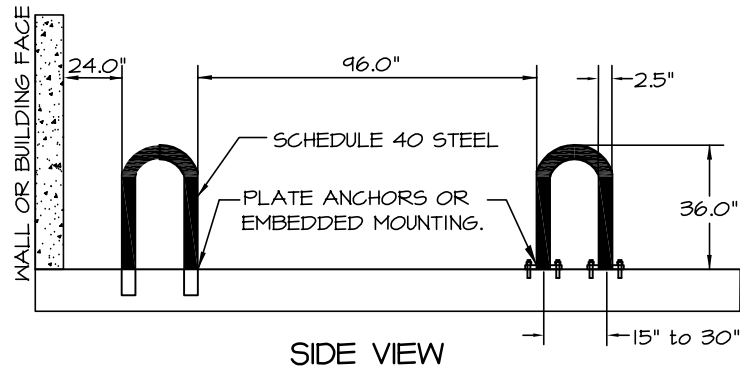
NOT TO SCALE



**TOWN OF STALLINGS  
LAND DEVELOPMENT STANDARDS**

**SPEED HUMP DETAIL**

STD. NO.	REV.
50.15	



**NOTES:**

1. BIKE RACKS SHOULD BE INSTALLED AS PER MANUFACTURER'S RECOMMENDED INSTALLATION PROCEDURES.
2. ALTERNATIVE BIKE RACKS OR LOCKERS MAY BE USED BUT ARE SUBJECT TO APPROVAL BY THE TOWN ENGINEER
3. ALL DIMENSIONS SHOWN ARE MINIMUM.

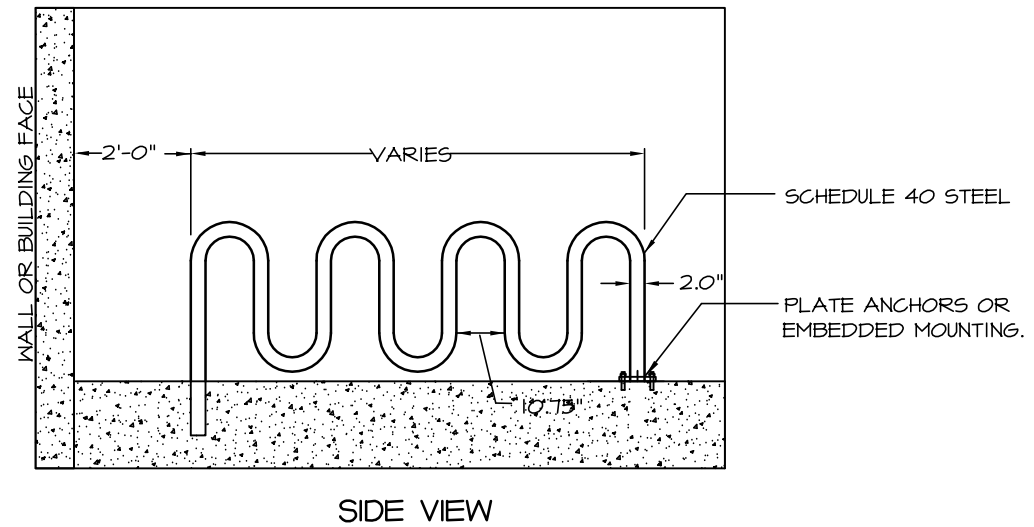
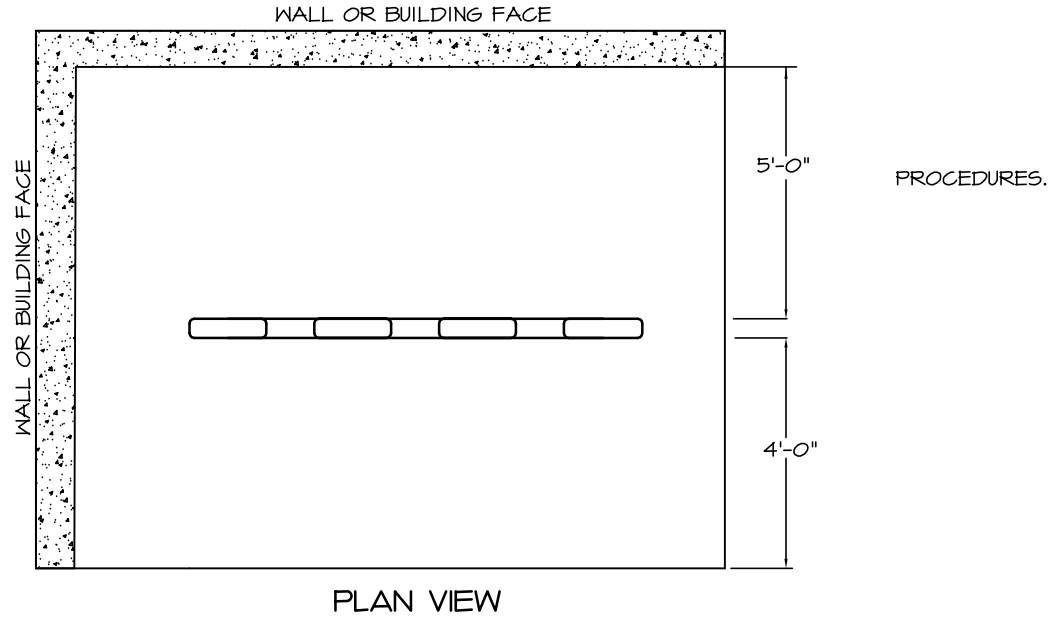
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**TOWN OF STALLINGS**  
**LAND DEVELOPMENT STANDARDS**

**INVERTED "U" RACK FOR  
 BICYCLE PARKING**

STD. NO.	REV.
50.20	



**NOTES:**

1. BIKE RACKS SHOULD BE INSTALLED AS PER MANUFACTURER'S RECOMMENDED INSTALLATION PROCEDURES.
2. ALTERNATIVE BIKE RACKS OR LOCKERS MAY BE USED BUT ARE SUBJECT TO APPROVAL BY THE TOWN ENGINEER.
3. ALL DIMENSIONS SHOWN ARE MINIMUM.

NOT TO SCALE



**TOWN OF STALLINGS**  
**LAND DEVELOPMENT STANDARDS**

**WAVE RACK FOR**  
**BICYCLE PARKING**

STD. NO.	REV.
50.21	